

# **HERITAGE IMPACT STATEMENT**

**128 & 130-150  
BUNNERONG ROAD,  
PAGEWOOD**

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# EXECUTIVE SUMMARY

Urbis has been engaged by Meriton Properties to prepare the following Heritage Impact Statement for the Planning Proposal for the subject site at 128 & 130-150 Bunnerong Road, Pagewood.

The site previously formed part of a larger site which was constructed for General Motors Holden and from the 1980s was occupied by British American Tobacco Australasia (BATA) facility. BATA have since vacated the site and only 40% of their former holding is now being retained for industrial use (freight storage).

The subject site has no associated heritage listings. However, there are built elements on the site which have been assessed by Urbis as having heritage significance (refer Section 4) including the former administration building in the north east corner. It is also located in the general vicinity of Jellicoe Park (Item 155) and Harris Reserve (Item 66).

Therefore, this Heritage Impact Statement has been prepared to assess the potential heritage impact of the proposed works on the identified heritage significance of the site and the proximate heritage items.

The following observations have been summarised from the assessment set out in Section 5:

- There are no construction works proposed as part of this application which only proposes the rezoning of the land. There would therefore be no physical impact on the early structures on the site as a result of this application;
- The planning proposal proposes to change the existing zoning of the site from IN1 General Industrial and R3 Medium Density Residential to R4 High Density Residential. The subject site currently sits in isolation as the only industrial development in a predominantly residential area. Specifically, the majority of the BATA site (that to the south) has already been rezoned and construction has started for its residential conversion. It should also be noted that the subject site is an area strategically accepted for a change in land use. The proposed rezoning is consistent with the strategic vision for Eastgardens as outlined in the Botany Bay Planning Strategy 2031, which envisaged a mixed-use centre in the long term, once BATA had vacated the land. As such, the subject site including the fabric of identified heritage significance and the proximate heritage items will inevitably exist in the context of larger scale development of a different typology than what exists today. This application proposes a residential rezoning which responds to the desired future character of the area in terms of scale and typology;
- While the rezoning would facilitate the end of the historic industrial use of the site it should be noted that the significance of the site is vested in its previous association with General Motors. The notable historic use therefore specifically constitutes the car manufacturing/assembly industry which moved off the site in the 1980s. The existing generic industrial use i.e. freight storage does not directly contribute to the significance of the site. The requirement to retain its existing industrial use is therefore diminished;
- The planning proposal allows residential development in the area currently occupied by buildings associated with the industrial use of the plant. It is understood that the retention of all early elements on site would not provide for the practical redevelopment of the site for residential use. It is considered acceptable that some of this fabric be removed to facilitate the desired use as, despite the historic associations, much of the early plant fabric was removed under DA2011/272 and the remnant plant building largely comprises standard sawtooth construction with significant areas of contemporary fabric resultant of its conversion into a light industrial facility;
- Cognisant of the aesthetic significance and scale of the former administration building to the north-east corner of the subject site, it is proposed to apply a 28m maximum building height along the northern boundary of the site. This lower scale in the immediate vicinity of the early fabric would serve to minimise visual dominance and to sympathetically moderate the difference in scale between it and the higher development to the south. The zoning across the remainder of the site would allow buildings up to 65m in discreet areas;
- It is proposed to increase to the maximum floor space ratio from 1:1 to 2.35:1. For the reasons above, it is considered that this may be achievable whilst retaining the identified intrinsic heritage significance. Support of the proposed maximum floor space ratio is contingent on the appropriate application of bulk across the site i.e. with appropriate setbacks from the heritage fabric. This will be finalised at master planning stage;
- There is no statutory requirement to retain any of the fabric on the site from a heritage perspective as the site has no statutory heritage listing. However, cognisant of the significance of some identified fabric on the site the concept master plan includes the retention of the administration building and

the two northern pillars to the former assembly plant which are the most intact structures on the site; and

- It is understood that the administration building and associated buildings down the eastern boundary of the site are intended to be dedicated to Council for ongoing community/public use. Public access to the site has historically been limited given its industrial use. It is therefore appreciated that transfer of this part of the subject site to public land would allow appreciation of the site's values.

In accordance with the above observations the planning proposal is supported from a heritage perspective.

# 1. INTRODUCTION

## 1.1. BACKGROUND

Urbis has been engaged by Meriton Properties to prepare the following Heritage Impact Statement for the planning proposal for the subject site at 128 & 130-150 Bunnerong Road, Pagewood.

The site previously formed part of a larger site which was constructed for General Motors Holden and from 1980 was occupied by British American Tobacco Australasia (BATA) facility. BATA have since vacated the site and only 40% of their former holding is now being retained for industrial use (freight storage). The area in blue in Figure 1 below is the subject of this assessment.

The subject site has no associated heritage listings. However, there are built elements on the site which have been assessed by Urbis as having heritage significance (refer Section 4) including the administration building to the north east. It is also located in the general vicinity of Jellicoe Park (Item 155) and Harris Reserve (Item 66).

Therefore, this heritage impact statement has been prepared to assess the potential heritage impact of the proposed works on the identified heritage significance of the site and the proximate heritage items.

## 1.2. SITE LOCATION

The site is located at 128 & 130-150 Bunnerong Road, Pagewood. It is legally described as the whole of Lot 1 in DP 1187426 and the northern portion of Lot 2. It has frontages to Bunnerong Road to the east, Heffron Road to the north and Banks Avenue to the west. It is located within the Bayside LGA.

Figure 1 – Aerial imagery showing the approximate boundaries of the subject site (blue boundary).



Source – Nearmaps.com

## 1.3. METHODOLOGY

This Heritage Impact Statement has been prepared in accordance with the NSW Heritage Branch guideline 'Assessing Heritage Significance' (2001). The philosophy and process adopted is that guided by the *Australia ICOMOS Burra Charter* 1999 (revised 2013).

Site constraints and opportunities have been considered with reference to relevant controls and provisions contained within the Botany Bay Local Environmental Plan 2013 and the Botany Bay Development Control Plan 2013.



Please note that the canteen building and the boilerhouse were not able to be accessed on the day of the site visit.

This report only addresses the planning proposal in detail. A detailed assessment of the physical and visual impacts of any new development on the site would be undertaken as part of future applications.

## **1.4. AUTHOR IDENTIFICATION**

The following report has been prepared by Alexandria Barnier (Senior Consultant).

Unless otherwise stated, all drawings, illustrations and photographs are the work of Urbis.

## **1.5. THE PROPOSAL**

The proposal includes:

- Change of zoning from IN1 – General Industrial and R3 – Medium Density Residential to R4 – High Density Residential;
- Increasing the existing maximum FSR (1:1) to 2.35:1; and
- Increasing the existing height of building (variously 32m, 28m, 21m and 17m) to part 28m and part 65m.

The concept master plan prepared by Hassell has been briefly assessed herein to determine any heritage impact likely to be facilitated by the planning proposal. The master plan includes residential flat buildings between 8 and 20 storeys high across the site (proposed no. of dwellings is 1977). There is a large community space bisecting the site. The concept master plan allows for the retention of the significant former administration building to the north-east corner of the site and it is understood that there is potential to retain or interpret the two remnant pillars of the plant building along the sites northern boundary.

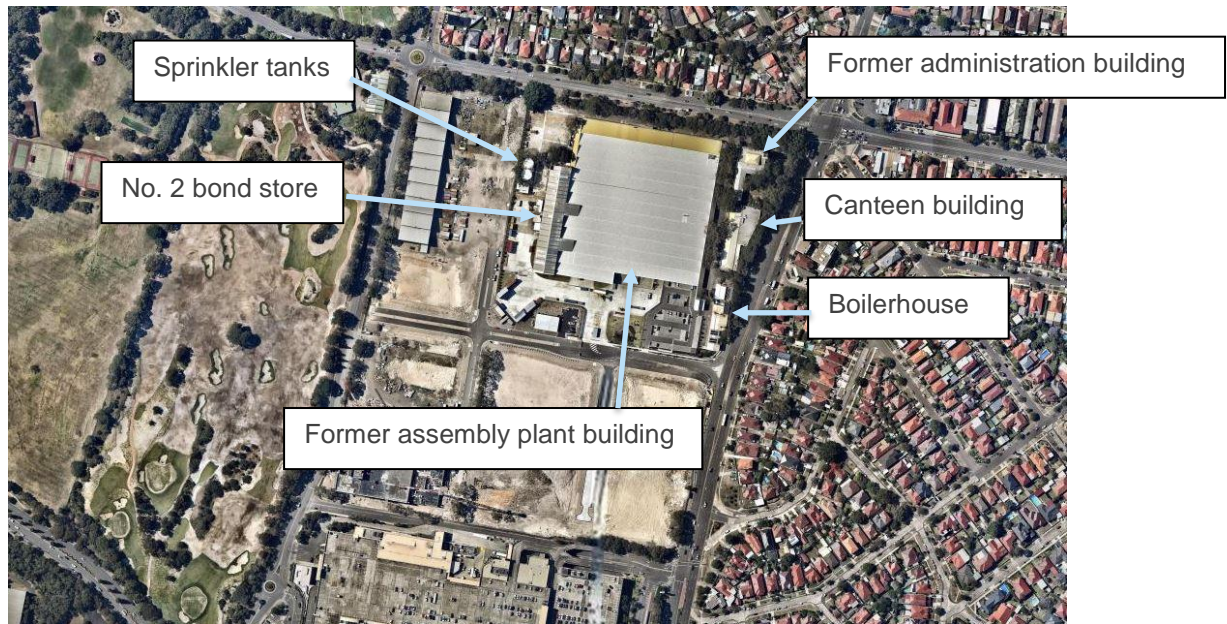
The following plans have been referenced in this report:

- Existing and proposed zoning maps, received by Urbis on 17.01.2017; and
- Concept Plan Urban Design Report, dated 27.03.2017.

## 2. SITE DESCRIPTION

The site is bounded by Heffron Road along the northern boundary, Bunnerong Road to the east and Banks Avenue to the west. An access road extends west from Bunnerong Road and defines the southern boundary of the site. It is one of three vehicular access points to the site. Vehicle access is also from the northern boundary and from the north-west corner. The site is largely occupied by open yard spaces and several remnant structures on the site which date from various periods of development. A description of the individual structures is set out below.

Figure 2 – Location of individual structures on the subject site.



### Administration Building

There are two original buildings to the north-east corner of the site. The most distinctive is the two storey administration building which was built in the Inter War Functionalist Style (completed 1940). This building has some presence from the adjacent intersection (Bunnerong and Heffron Road) largely characterised by its clock tower with inset teal tiles. The horizontality of the building is emphasised by its rhythmic fenestration pattern and the pronounced string courses which run at the window sill height and the ceiling height of both floors. There is also a string course running along the top of the parapet.

There have been some changes to the primary façade of the administration building. A number of doors and small windows have been added which have had some impact on the legibility of the original design intent of the façade. There is also a large yellow bulkhead and ground floor ceiling height running part way along the façade. There is a WWI memorial tree and plaque in the setback from Bunnerong Road adjacent to the building.

Internally, the building appears to largely retain its original layout. It also retains its original timber office partitions. Otherwise, it appears that the building has largely been refit in line with its use by BATA and now comprises largely unremarkable fabric. Further investigation should be undertaken to determine whether original finishes are present under the existing.



Figure 3 – External images of the administration building.



Picture 1 – View west towards the principal eastern façade.



Picture 2 – Clocktower to the north-eastern corner.



Picture 3 – View towards the eastern façade.



Picture 4 – View towards the southern western corner.



Picture 5 – View of the southern façade

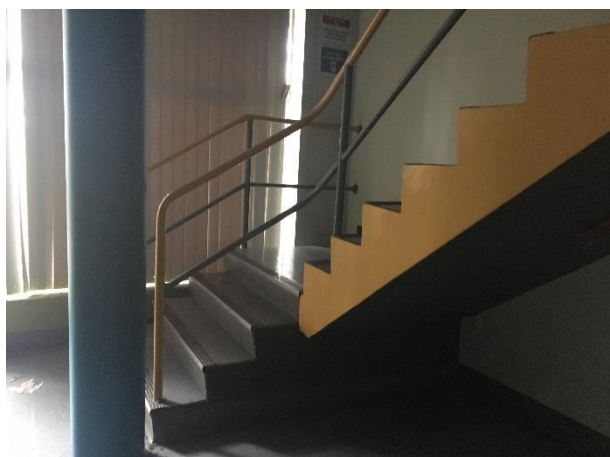
Figure 4 – Internal images of the former administration building.



Picture 6 – View north up ground floor hallway.



Picture 7 – View across ground floor office space.



Picture 8 – Original stairs from ground floor.



Picture 9 – View across northern space. Assumed to be former showroom.

## The Canteen Building

The Canteen Building is located to the immediate south of the administration building. It was constructed in 1940 in the same style as the aforementioned however it is a single storey. The face brick building originally had a double storey entry feature element halfway down its eastern façade however this was removed in the 1950s when the building was expanded eastward towards Bunnerong Road. It was also extended to the north towards the administration building. There are unsympathetic awning structures to the northern and western facades. The building is therefore less remarkable than it originally was and makes a lesser contribution to the setting of the distinctive building described above.

Access to the internal spaces within this building was not possible on the day of the site inspection. It appears from looking through the windows that the building has been refit, similar to that above. Access should be gained and further investigation undertaken to determine extent of original finishes.



Figure 5 – External images of the canteen building.



Picture 10 – View towards northern façade of 1950s extension to the canteen building.



Picture 11 – View to the north-western corner.



Picture 12 – View along the western façade.



Picture 13 – View along part of the northern façade.



Picture 14 – View down eastern façade (1950s extension).

Figure 6 – Internal view of canteen building (through window).



Picture 15 – View across area in original section.

### The Boiler House

The Boiler House building is located to the south of the Canteen Building. It postdates the earlier buildings, first appearing in historic aerials in 1950. The building is largely utilitarian in character with few qualities representative of an identifiable period. The building has a gable ended roof form. The fenestration is multipaned warehouse style and there are four vehicle entries along the western façade. There are a number of service risers from the roof. Access to this building was not permitted on the day of the site inspection.

Figure 7 – External images of the Boiler House.



Picture 16 – View east towards the boilerhouse.





Picture 17 – View to the north west corner.



Picture 18 – View towards the south west corner.



Picture 19 – View along the western façade.



Picture 20 – View of the ancillary structures to the north of the Boiler House.

### Former Assembly Plant Building

This building was originally a sawtooth structure marked at the corners by a face brick pillar with inset teal tiles. The sawtooth structure was added to considerably throughout the history of the site however much has now been removed, with less than half remaining. Much of the early structure has been removed with less than half the sawtooth bays remaining. The two original pillars on the northern boundary are extant. The original fenestration along all facades has been replaced with contemporary fabric.

Internally it appears that the original sawtooth structure is relatively intact however the cladding over may have been replaced. Further investigation should be undertaken to determine if any machinery associated with General Motors has been retained, none was identified during the site visit.

Figure 8 – External images of the plant building.



Picture 21 – View along eastern façade.



Picture 22 – Existing presentation of the assembly plant to the north-east



Picture 23 – View to the north-west corner.

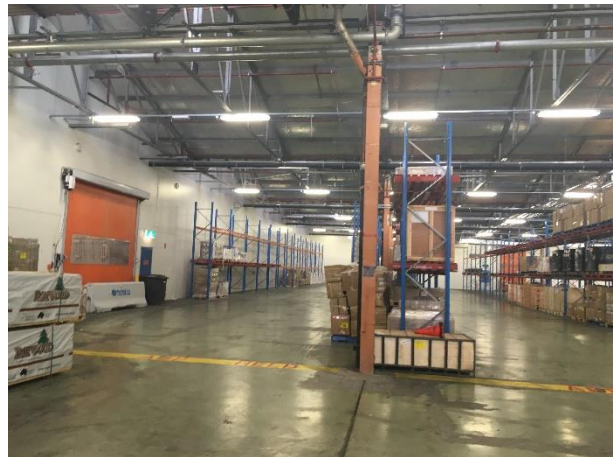


Picture 24 – View along northern facade.

Figure 9 – Internal images of the assembly plant building.



Picture 25 – View east across plant building.



Picture 26 – View north across plant building.



## No 2. Bond Store

To the west of the main factory building is the no. 2 Bond Store. The bond store dates from the 1940s and retains an original northern façade. However, the store has been substantially expanded south and the rear façade has been removed and replaced with contemporary wall sheeting. The northern façade is constructed of brick and is of the same character as the pillars on the building adjacent. It features face brick with inset tiles.

The original internal roof structure appears original and constitutes a series of steel trusses.

Figure 10 – External images of the No. 2 Bond Store.



Picture 27 – Remnant northern façade.



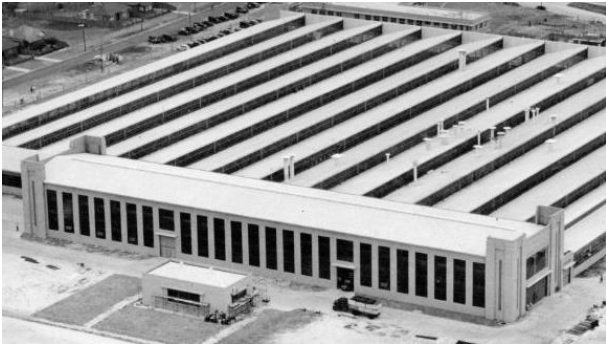
Picture 28 – View towards the north west corner.



Picture 29 – View along western façade.



Picture 30 – View toward north west corner.



Picture 31 – Original presentation to the south west.



Picture 32 – Existing presentation to the south west.

Figure 11 – Internal images of the no. 2 bond store.



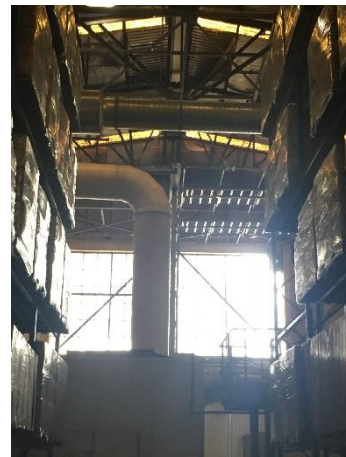
Picture 33 – View across bond store.



Picture 34 – Original roof structure.



Picture 35 – View north towards inside of original façade.



Picture 36 – View north towards inside of original façade.

## Sprinkler Water Tanks

To the immediate west of the No.2 Bond Store is located two sprinkler water tanks. These appear to be of standard construction and were built between 1954 and 1963. The tanks are partly visible from Heffron Road as seen in the images below.

Figure 12 – Images towards the sprinkler water tanks.



Picture 37 – View south from Heffron Road.



Picture 38 – View south from Heffron Road.

*Source: Google Earth*



## 3. HISTORICAL OVERVIEW

### 3.1. AREA HISTORY

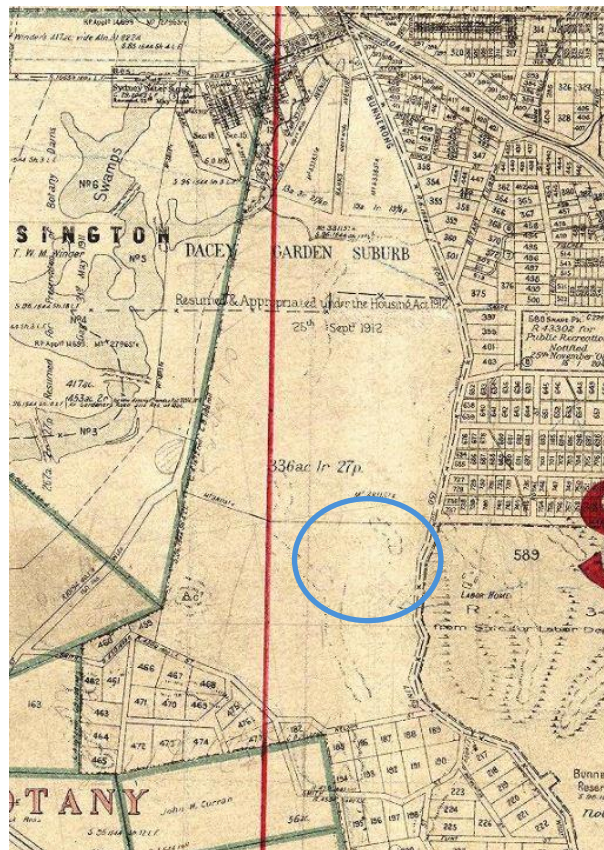
Pagewood was originally planned as a garden suburb, similar to neighbouring Daceyville. However, it was developed to provide housing after World War I when there was a shortage of land. The new suburb began developing in 1919 as an estate called Monash Gardens. The suburb was named Pagewood in 1929 to honour Alderman F.J. Page of Botany Council.<sup>1</sup>

### 3.2. EARLY DEVELOPMENT OF THE SUBJECT SITE

The plant occupies an original site which comprised an area of 25 acres at the corner of Bunnerong and Maroubra Bay Roads. The ground formation before the construction of the subject buildings was described as consisting of sand to an unknown depth with a strata of 'Waterloo' rock typical of the area.

Development on the subject site appears to have been slow. The early map of the Parish of Botany in Figure 13 below indicates that there was no development on the subject site by this time and that it was not subdivided. It is possible that it had some agricultural uses, however ownership of the land was not formalised until the 1930s. Refer to Table 1 which sets out the original land grants which eventually comprised the General Motors Holden site.

Figure 13 – 1916. Botany Parish Map indicating the approximate extents of the subject site (circled blue).



Source: Land and Property Information

Table 1 – Early land grants which comprised the eventual General Motors Holden site.

Name of Portion	Area of Portion	Name of Grantee	Date of Grant	Reference
2463	18 and ¼ perches	Emily Martha Seabrook	22/8/1930	Vol.4429 fol. 147
2457	18 and ¼ perches	Ellen Eliza Rhodes	12/6/1931	Vol. 4486 fol. 143
2456	18 and ¼ perches	Robert Rhodes	12/6/1931	Vol. 4486 fol. 144
2435	7 and ¼ perches	Raymond Stubbs	1/9/1933	Vol. 4590 fol. 113
2445	9 perches	Horace Beeton	17/10/1933	Vol. 4594 fol. 174
2433	7 and ¼ perches	Frederick John Hillman	26/7/1934	Vol. 4636 fol. 116
2454	7 and ¾ perches	Henry Collen Wheelen	30/9/1935	Vol. 4716 fol. 135
2455	17 and ¼ perches	Angus Nugent	13/10/1936	Vol. 4795 fol. 218
2460	18 and ¼ perches	Alma Vera Abassden	27/7/1937	Vol. 4859 fol. 130
3497	25 acres	General Motors Holden	31/3/1939	Vol. 5028 fol. 158

### 3.3. GENERAL MOTORS HOLDEN

Land for GMHs Pagewood Plant, was purchased by the company in July 1939<sup>2</sup>. However, newspaper records indicate that construction had already begun on the plant by June 3<sup>rd</sup> 1939.<sup>3</sup> The plant cost 270,000 pounds with only around 10,000 pounds being used outside the Commonwealth.<sup>4</sup> The plant was constructed in a record 7 and a half months. It used 1,000 tons of Australian steel and had heat absorbing windows covering an area equal to one and a half acres. The existing General Motors Holden (GMH) factory was officially opened on February 15<sup>th</sup> 1940 by then Prime Minister Robert Menzies.<sup>5</sup>

<sup>2</sup> Certificate of Title Vol. 5056 Fol. 10

<sup>3</sup> Cootamundra Herald, 8/1939

<sup>4</sup> Cootamundra Herald, 8/1939

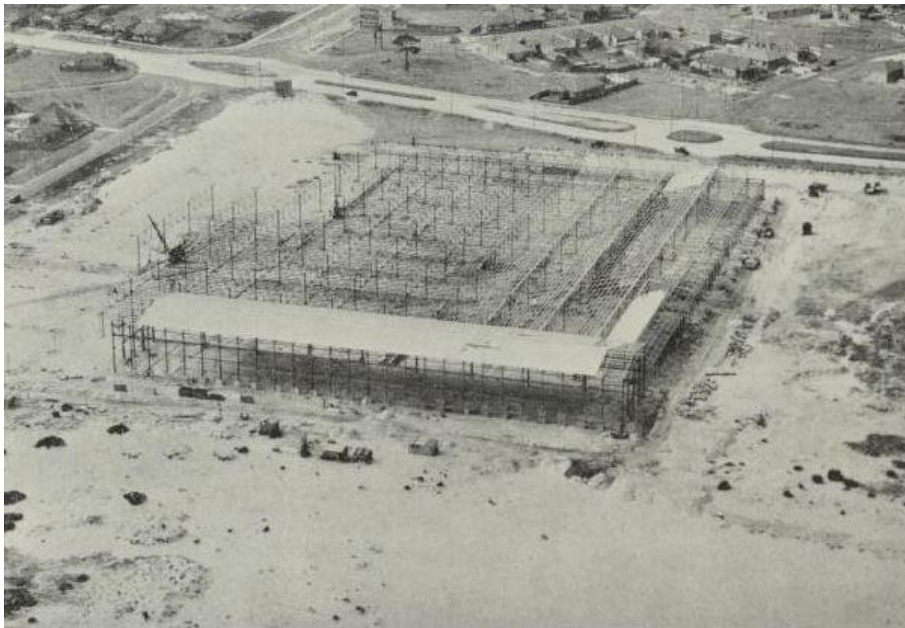
<sup>5</sup> Truth, 2/1940

Figure 14 – March 1939. Empty site before construction of the plant.



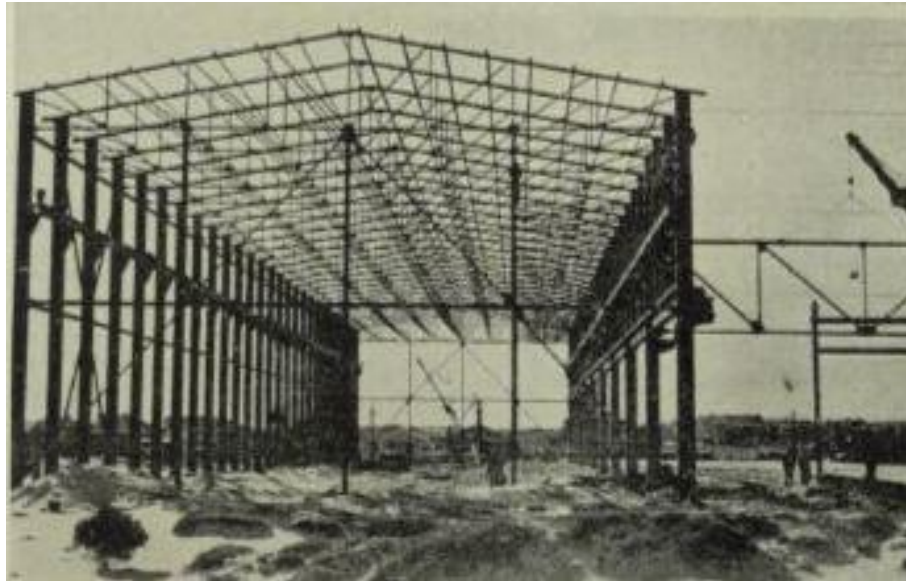
Source: *Construction*, 3/1940

Figure 15 – March 1940. Construction of the roof of the plant.



Source: *Building*, 3/1940

Figure 16 – Undated. Construction of the warehouse building.



Source: *Building*, 3/1940

Figure 17 – Undated. Construction of the warehouse building.



Source: *Building*, 3/1940

The consulting engineer for the Company was Mr Gibson and the builders were Concrete Constructions Pty. Ltd. At this time it was one of a group of factories associated with the company, one in each capital city except Hobart.

The foundations had industrial reinforced concrete foundations for each structural steel column. All exposed brick work in all buildings, fences, retaining walls and elsewhere was supplied by Wunderlich's "Colortex" texture bricks, of a specially selected buff colour. Over 50 types of specially moulded texture bricks were supplied for the works.

Around 320 contractors and sub-contractors were engaged in the construction of the plant. Some companies involved in the construction of the plant included:

- Roofing - James Hardie & Co. Pty. Ltd. Fibrolite and Wunderlich Limited's Durasbestos.
- Concrete flooring – Melocco Bros. Pty. Ltd.
- Slagwool insulation – Bradford Insulation Pty. Ltd.
- Ceiling plaster – Art. Plasto Pty. Ltd.
- Paints and varnishes – Sherwin-Williams Co. (Aust).
- Lighting brackets – British General Electric Co. Pty. Ltd.



### Administration Building

The two storey Administration building at the corner of Heffron Road and Bunnerong Road was constructed with 18,267 square feet of floor space and housed Manufacturing, Sales, Finance, Audit, IDEC, Service and Traffic personnel. The roof comprised structural steel trusses and purlins, with cement asbestos corrugated roofing and box gutters. The tower to the north-east corner of the administration building was described in the March 1940 Building magazine as follows:

*"Rising like a gigantic modern sentinel, the tower at the corner of the Administrative Building indicates the presence of the new General Motors- Holden's assembly plant from afar. Clear cut in design and silhouetting against the sky the tower symbolises the progressiveness of the organisation which it dominates. Wunderlick Limited's Colortex bricks of a buff tone were employed, while Agee glass bricks constitute the vertical panels."*

The arrangement of the windows and internal columns was designed to allow the subdivision of the internal offices in units of three feet in width between cross partitions. All windows to offices on the west side were in Coldlite non actinic glass, other office windows were in sheet glass and windows to the showroom was polished plate glass. The concrete ground floor was covered with Tasmanian Oak, marbled buff linoleum on a bituminous felt underlay was provided in all offices. Internally there was a dado to window sill height carried out in Queensland Maple veneer panels, nailed to Oregon timber grounds.

The structure was specifically designed for the future installation of complete air conditioning and a concrete tunnel was constructed under the roadway between the building and the Assembly Plant where the necessary equipment would be installed.<sup>6</sup>

The administration building featured a showroom in the north-east corner which had main stairs accessing the first floor where the executive offices were accommodated. A turn table was included in the showroom floor (refer Figure 26). The facing of the walls of the main stairs opening out to the Showroom is in Scagliola by Melocco Bros. Pty. Ltd. with Verona marble skirtings, stringers, trims and to the ends of the treads and risers. Special ceiling lighting was provided in the showroom for the illumination of the cars displayed on the first floor to be viewed from the outside by the public at night.

A servery was provided on the first floor. Lavatories and cloakroom accommodation for males was located on the ground floor and for females on the first floor. A projection room for the showing of cinematograph films was provided on the first floor, with accommodation for approximately 50 people.<sup>7</sup>

### Club House/Canteen Building

Also, constructed in the initial phase was the club house/canteen building to the south. The club house was erected at a cost of 75,000 pounds and was designed specifically for the welfare and comfort of the staff and employees. It comprised a gas meter room, gardener's store, sports store, library, cloakrooms, lavatories, office for social secretary, dining room and kitchen. Accommodation for 300 was provided and the building could be employed for concerts, recreational purposes and social functions in addition to its function as a cafeteria. The tables were of Queensland Maple, with sheet leather tops. The library was equipped with pressed steel shelving.

The ground floor throughout was of concrete laid on the solid, with hardwood joists embedded at 16" centres. The floor was of tallow wood, sanded and wax polished. The internal wall finish was of setting plaster with Queensland Maple veneer dado 6' high in the dining room and passages. The ceiling over the offices and kitchen was of similar construction to those over the first floor in the administration building.

Tennis courts were provided adjacent. A concrete pergola was erected to connect this building with the administration building and a similar one was erected at the southern end of the structure. On three sides of the building, the paving of the paths and terrace was done in flagstone paving.

Additions to the Canteen building to the south in 1952 (cost 109,000 pounds) increased its floor area and seating capacity.<sup>8</sup>

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<sup>6</sup> Building, 3/1940

<sup>7</sup> Ibid.

<sup>8</sup> Ibid.



### Assembly Plant and Warehouse Building.

The main plant with distinctive corner pillars and the no. 2 bond store (original warehouse building) were also constructed in the original phase of development. Offices in the main factory building housed the personnel department, supply and engineering in addition to manufacturing personnel. The structural steel frame of the main factory building consists of sawtooth construction, 500'x520', with a warehouse annexe 360'x65' 6' centre to centre measurements -total area was 283,400 square feet. The structural steelwork throughout was bolted, with the exception of certain heavy girders adjacent to the warehouse. Erection was carried out by two steam railway cranes.<sup>9</sup>

Figure 18 – c1940. GMH plant nearing completion viewed from south west.



Source: <http://www.hrc.org.au/memories/photographs.html>

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<sup>9</sup> Ibid.

Figure 19 – 1940. GMH (General Motors Holden) viewed from the north-east.



Source: *GMH People* Volume 15 Number 7 1963

Figure 20 – c1940. GMH (General Motors Holden) viewed from the north.



Source: <http://www.hrc.org.au/memories/photographs.html>

Figure 21 – 1940. First official photo taken after the completion of the plant.



Source: <http://www.hrc.org.au/memories/photographs.html>

The first car was completed in the new plant on January 19<sup>th</sup> 1940.<sup>10</sup> For a brief period the plant assembled Vauxhall, Chevrolet, Pontiac and Bedford Vehicles before it was converted for defence needs. One of the first jobs for the plant was producing equipment for military use such as 25 pounder field pieces, wings for De Havilland Mosquito and munitions.

After the war the plant manufactured Frigidaire refrigerators and resumed the assembly of GMH vehicle products. Holden assembly started at Pagewood with the introduction of the all Australian Car in November, 1948. The Pagewood plant was one of the first plants to produce Holdens.

In 1956, Frigidaire operations were transferred to Dandenong and the body and vehicle assembly activities were further expanded.

Between the 40s and 60s the headquarters grew from a small 98,000sq. ft. Assembly Plant on a 6 ½ acre site to the largest GMH body and vehicle and assembly plant. During the 1950s and 1960s Pagewood was expended with new and bigger buildings. Specifically, the primary sawtooth structure was expanded in 1952, 1955<sup>11</sup> and again in 1959<sup>12</sup>. The expansion in 1952 saw the plant expanded from 283,000sq ft to 447,745 sq ft under an 11-million-pound expansion programme.<sup>13</sup> By 1954 the rear (southern) façade of the no. 2 bond store had been removed to allow for the expansion of the factory (refer Figure 37).

By 1963 the plant covered a built area of 15.65 acres on a 34 acre site and an additional 19 acres had been acquired adjacent for further development. At that time it had the yearly capacity to assemble, paint and trim 65,280 bodies and produce 55,680 complete vehicles.

In 1980 the plant was closed during Holden's restructuring and rationalisation of plants throughout Australia. The Pagewood plant produced the 1,500,000 and 3,000,000<sup>th</sup> Holdens.

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<sup>10</sup> Construction, 3/1940

<sup>11</sup> The Inverell Times, 8/1954

<sup>12</sup> The Cumberland Argus, 4/1958

<sup>13</sup> The Sunday Herald, 6/1953

Figure 22 – 1940. GMH (General Motors Holden) opening with Robert Menzies, Prime Minister (1939-1941, 1949-1966)



Source: <http://www.hrc.org.au/memories/photographs.html>



### 3.4. HISTORIC IMAGES

Figure 23 – 1940. GMH (General Motors Holden) opening with Robert Menzies, Prime Minister (1939-1941, 1949-1966)



Source: Sam Hood. Available at - State Library of New South Wales

Figure 24 – 1940. View towards north-east corner of the GMH (General Motors Holden) site at opening 1940.



Source: Sam Hood. Available at - State Library of New South Wales

Figure 25 – 1940. View towards north-east corner of GMH (General Motors Holden) site at opening 1940.



Source: Sam Hood. Available at - State Library of New South Wales

Figure 26 – March 1940. View across showroom to north-east of the administration building.



Source: Building, 3/1940

Figure 27 – 1940. View south along the east boundary of GMH (General Motors Holden) site at opening 1940.



Source: Sam Hood. Available at - State Library of New South Wales

Figure 28 – 1940. View west along northern boundary of GMH (General Motors Holden) site at opening 1940.



Source: Sam Hood. Available at - State Library of New South Wales



Figure 29 – March 1940. View west across the plant building showing the first delivery of Chevrolet cars.



Source: *Building*, 3/1940

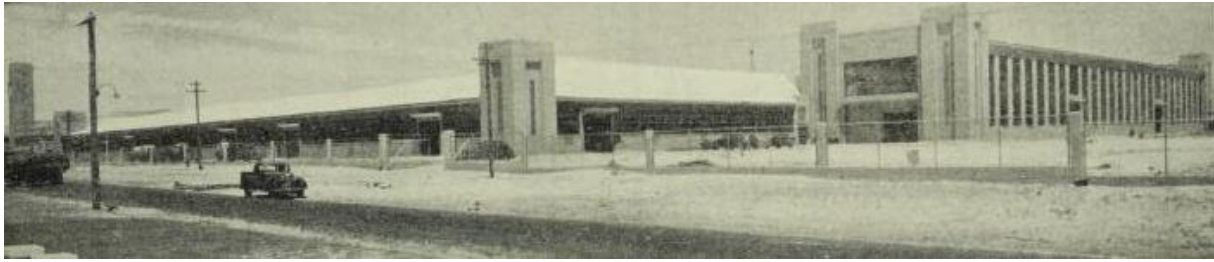
Figure 30 – 1940. View north west towards main building at GMH (General Motors Holden) site at opening 1940.



Source: Sam Hood. Available at - State Library of New South Wales

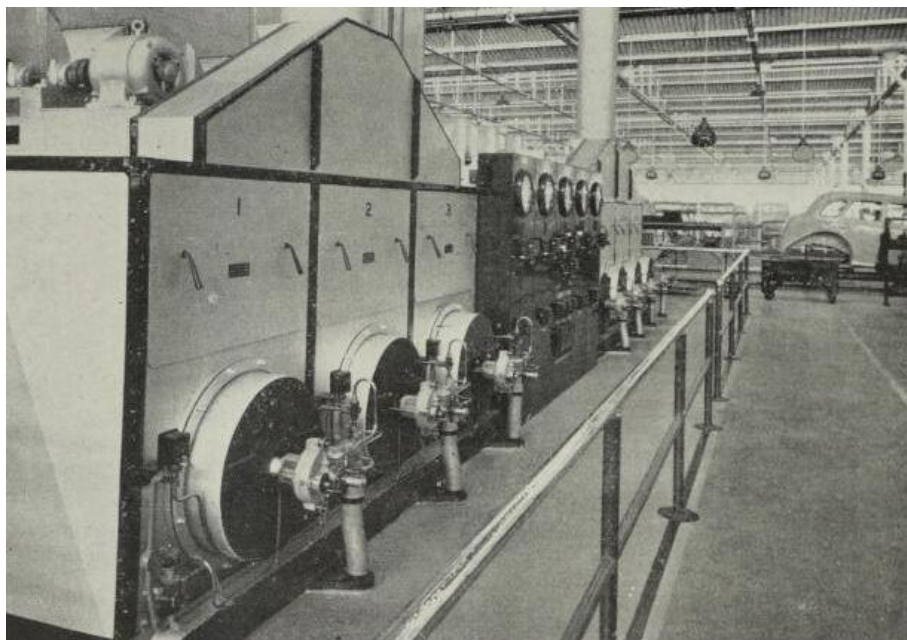


Figure 31 – 1940. View south east towards plant and warehouse buildings.



Source: *Building*, 3/1940

Figure 32 – March 1940. View south across plant showing original oil fired heat interchangers.



Source: *Building*, 3/1940

Figure 33 – 1940. View west towards GMH (General Motors Holden).



Source: Sam Hood. Available at - State Library of New South Wales

Figure 34 – 1940. Building south of the main administration building before alterations.



Source: Sam Hood. Available at - State Library of New South Wales

Figure 35 – c1943. Employees of De Havilland Aircraft Pty Ltd at GMH Pagewood NSW painting camouflage colours, one of the final stages of wing assembly for DH98 Mosquitos.



Source: <http://www.hrc.org.au/memories/photographs.html>

Figure 36 – c. 1950. Foreman at lunch in Canteen Foreman's Room.



Source: <http://www.hrc.org.au/memories/photographs.html>

Figure 37 – May 1954. Aerial of the site before various alterations to the main factory and Club House.



Source: <http://www.hrc.org.au/memories/photographs.html>

Figure 38 – 1950. Building south of the main administration building before alterations.



Source: State Library of New South Wales



Figure 39 – c1956. Hard Trim Inspection at end of HT line.



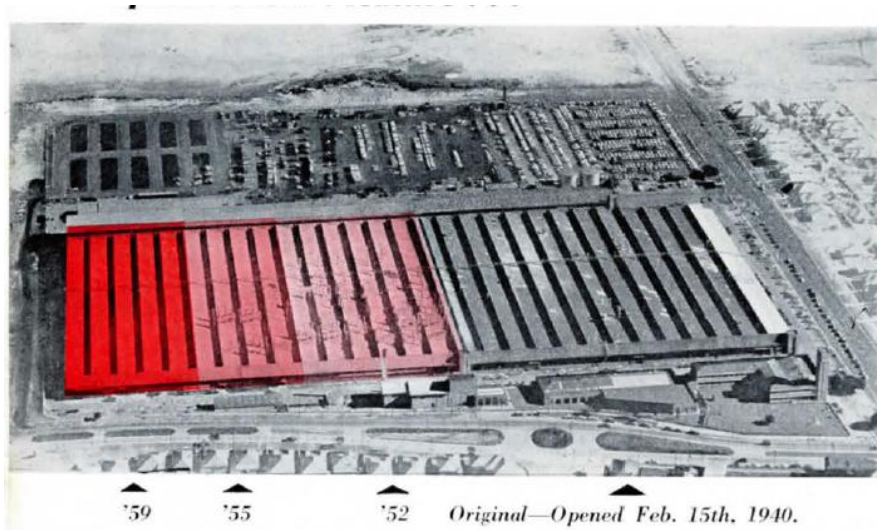
Source: <http://www.hrc.org.au/memories/photographs.html>

Figure 40 – 1958. Completion of the 500,000 Holden.



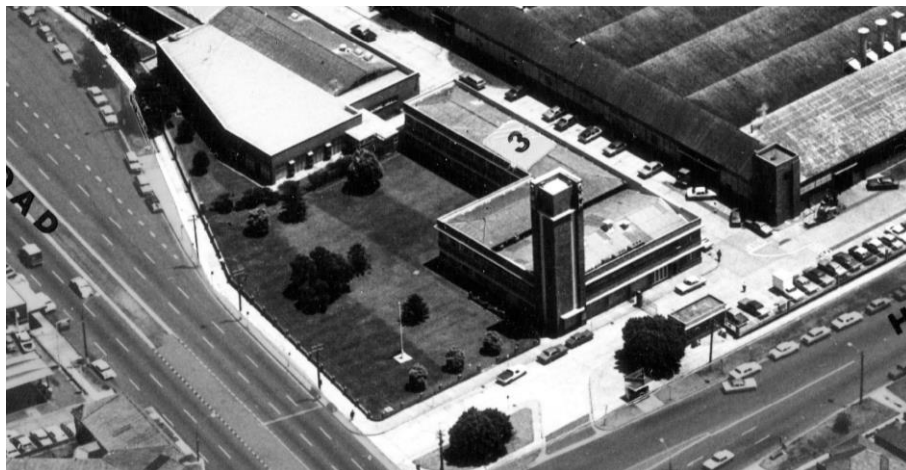
Source: <http://www.hrc.org.au/memories/photographs.html>

Figure 41 – 1963. Graphic showing the extensions to the GMH plant.



Source: *GMH People* Volume 15 Number 7 1963

Figure 42 – Aerial view of GMH Pagewood Plant.



Source:  
[HTTP://WWW.HRC.ORG.AU/IMAGES/MEMORIES/NEIL%20POGSON/PAGEWOOD%20AERIAL%20BIT600B&W.JPG](http://www.hrc.org.au/images/memories/neil%20POGSON/PAGEWOOD%20AERIAL%20BIT600B&W.JPG)

Figure 43 – Aerial view of GMH Pagewood Plant.



Source:

[HTTP://WWW.HRC.ORG.AU/IMAGES/MEMORIES/NEIL%20POGSON/PAGEWOOD%20AERIAL%20BIT600B&W.JPG](http://www.hrc.org.au/images/memories/neil%20POGSON/PAGEWOOD%20AERIAL%20BIT600B&W.JPG)

### 3.5. CLOSURE OF THE PLANT

A total of 1,200 workers lost their jobs when the plant was closed. They were offered work at three other plants in Australia. The decision was made as the plant was considered outdated and the upgrade costs were estimated to have been over \$100 million.<sup>14</sup>

### 3.6. BRITISH AMERICAN TOBACCO AUSTRALIA

Since 1982 the plant has been used to manufacture tobacco products for British American Tobacco Australia (BATA). The site was sold by BATA in April 2015 for \$90 million.<sup>15</sup>

### 3.7. DATE OF CONSTRUCTION

The original section of the plant was opened in February 1940. Later additions are as set out below.

### 3.8. ALTERATIONS AND ADDITIONS

The following table constitutes a record of the development applications relevant to the site which are held by Bayside Council and which are evident from the graphic evidence.

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<sup>14</sup> State Library of South Australia, GMH Pagewood Plant closure (video recording) transcript

<sup>15</sup> RPdata.

Table 2 – Record of applications for alterations and additions.

DATE	WORKS	REFERENCE
1952	Extension of the sawtooth structure south from the original.	
1952	Extension of the canteen in the administration building. Cost of 190,000 pounds.	
1955	Extension of the sawtooth structure south.	
1959	Extension of the sawtooth structure south.	
2000	Internal modifications to existing commercial and industrial buildings.	00/404
2002	Change of use to part of the existing factory building from a tobacco production area to be a tobacco storage area.	02/147
2003	Replacement of existing hot and cold servery equipment and erection of new partition wall to existing staff cafeteria.	04/226
2011	<p>Development Application for alterations and additions to the existing industrial development including:</p> <ul style="list-style-type: none"> <li>• Partial demolition of the existing factory buildings and structures to facilitate the reduction of the existing tobacco manufacturing operations to the north-east portion of the site;</li> <li>• External alterations and additions and the internal fitout of the remaining factory buildings;</li> <li>• Subdivision of the subject site into two (2) lots; and</li> <li>• Construction of an internal road to connect to Bunnerong Road, at 128 Bunnerong Road, Pagewood.</li> </ul>	11/272



## 4. HERITAGE SIGNIFICANCE

### 4.1. WHAT IS HERITAGE SIGNIFICANCE?

Before making decisions to change a heritage item, an item within a heritage conservation area, or an item located in proximity to a heritage listed item, it is important to understand its values and the values of its context. This leads to decisions that will retain these values in the future. Statements of heritage significance summarise a place's heritage values – why it is important, why a statutory listing was made to protect these values.

### 4.2. SIGNIFICANCE ASSESSMENT

The Heritage Council of NSW has developed a set of seven criteria for assessing heritage significance, which can be used to make decisions about the heritage value of a place or item. There are two levels of heritage significance used in NSW: state and local.

The following assessment of heritage significance has been prepared in accordance with the 'Assessing Heritage Significance' (2001) guides.

Table 3 – Assessment of heritage significance

Criteria	Significance Assessment
<b>A – Historical Significance</b> <i>An item is important in the course or pattern of the local area's cultural or natural history.</i>	<p>The subject site generally has historic significance. The phases of development on the subject site represent the growth of the car manufacturing/assembly industry in Australia. Specifically, the substantial initial phase of construction and subsequent phases indicate the steady growth of General Motors Holden through the middle of the century. By the 1960s the plant had become the biggest of its type for the company in Australia. The site generally therefore constitutes a prominent example of the operations of General Motors Holden in the middle of the 20<sup>th</sup> century.</p> <p>Notwithstanding this, although the site retains a general industrial use at present, the specific, continuous connection to the automotive industry was severed in the 1980s when the site was sold to BATA. Further, it is anticipated that key identifiers of the specific historic use of the place e.g. machinery related to the manufacture/assembly of cars has been removed in line with the later use as a tobacco manufacturing plant and then a freight storage facility.</p>
<b>Guidelines for Inclusion</b> <ul style="list-style-type: none"><li>shows evidence of a significant human activity <input type="checkbox"/></li><li>is associated with a significant activity or historical phase <input checked="" type="checkbox"/></li></ul>	<b>Guidelines for Exclusion</b> <ul style="list-style-type: none"><li>has incidental or unsubstantiated connections with historically important activities or processes <input type="checkbox"/></li></ul>

Criteria	Significance Assessment
<ul style="list-style-type: none"> <li>maintains or shows the continuity of a historical process or activity <input type="checkbox"/></li> </ul>	<ul style="list-style-type: none"> <li>provides evidence of activities or processes that are of dubious historical importance <input type="checkbox"/></li> <li>has been so altered that it can no longer provide evidence of a particular association <input type="checkbox"/></li> </ul>
<p><b>B – Associative Significance</b></p> <p><i>An item has strong or special associations with the life or works of a person, or group of persons, of importance in the local area's cultural or natural history.</i></p>	<p>The site generally has significance for its associations with General Motors Holden, an Australian company which arguably dates from 1856 when it was a saddlery company begun in South Australia. However, it should be noted that this association was not particularly long (just over 40 years) and that it ceased with the purchase of the site by BATA.</p>
<p><b>Guidelines for Inclusion</b></p> <ul style="list-style-type: none"> <li>shows evidence of a significant human occupation <input type="checkbox"/></li> <li>is associated with a significant event, person, or group of persons <input checked="" type="checkbox"/></li> </ul>	<p><b>Guidelines for Exclusion</b></p> <ul style="list-style-type: none"> <li>has incidental or unsubstantiated connections with historically important people or events <input type="checkbox"/></li> <li>provides evidence of people or events that are of dubious historical importance <input type="checkbox"/></li> <li>has been so altered that it can no longer provide evidence of a particular association <input type="checkbox"/></li> </ul>
<p><b>C – Aesthetic Significance</b></p> <p><i>An item is important in demonstrating aesthetic characteristics and/or a high degree of creative or technical achievement in the local area.</i></p>	<p>There are a number of elements comprised within the site which date from the original construction of the plant and have characteristics of the Inter War functionalist style. These include the administration building, the canteen building to its south, the main factory building and the no. 2 bond store.</p> <p>Notwithstanding, all elements have been significantly altered/added to. Removal of original elements include: two of the pillars to the main factory building; the southern façade of the no. 2 bond store; and the original detailed walls of the bond store. The original fenestration around the assembly plant has been altered and now comprises largely contemporary fabric. The original double height entrance feature to the canteen building was removed to allow for a substantial addition and has left the building without any particularly remarkable features of the style in which it was built except for its general horizontality.</p>

Criteria	Significance Assessment
	<p>Further, none of the original buildings have the same strong streetscape presence as originally intended due to the realignment of the roads (particularly Heffron Road) which have resulted in a substantial setback of all buildings from the street.</p> <p>Notwithstanding the above, the two-storey administration block to the northern eastern corner maintains its key characteristic features including horizontally proportioned fenestration patterns, expressed string courses and a strong vertical element. This vertical element constitutes the brick clock tower with tile insets to the north-east corner. The clock tower has a presence from the adjacent intersection and is a key identifier of the site. It is considered that this is the most identifiable and intact element on the site.</p> <p>Further, although the plant building appears to be a of a standard sawtooth design and the facades have been substantially altered, the remnant corners pillars associated with the plant building are indicative of the quality of the plant, have a visual relationship with the administration building and are appreciable from the public domain.</p> <p>The administration building and intact fabric associated with the plant building are therefore considered to have aesthetic significance.</p>
<p><b>Guidelines for Inclusion</b></p> <ul style="list-style-type: none"> <li>▪ shows or is associated with, creative or technical innovation or achievement <input type="checkbox"/></li> <li>▪ is the inspiration for a creative or technical innovation or achievement <input type="checkbox"/></li> <li>▪ is aesthetically distinctive <input checked="" type="checkbox"/></li> <li>▪ has landmark qualities <input type="checkbox"/></li> <li>▪ exemplifies a particular taste, style or technology <input checked="" type="checkbox"/></li> </ul>	<p><b>Guidelines for Exclusion</b></p> <ul style="list-style-type: none"> <li>▪ is not a major work by an important designer or artist <input type="checkbox"/></li> <li>▪ has lost its design or technical integrity <input type="checkbox"/></li> <li>▪ its positive visual or sensory appeal or landmark and scenic qualities have been more than temporarily degraded <input type="checkbox"/></li> <li>▪ has only a loose association with a creative or technical achievement <input type="checkbox"/></li> </ul>
<p><b>D – Social Significance</b></p> <p><i>An item has strong or special association with a particular community or cultural group in the local area for social, cultural or spiritual reasons.</i></p>	<p>It is appreciated that the site is likely to have significance to those who worked for General Motors in the mid 20<sup>th</sup> century. However, it should be considered that with the divestment of the site to BATA and the subsequent sale to a non-industrial entity means that after the lifetime of previous</p>

Criteria	Significance Assessment
	employees there is no potential for continued associations in this regard.
<b>Guidelines for Inclusion</b> <ul style="list-style-type: none"> <li>is important for its associations with an identifiable group <input type="checkbox"/></li> <li>is important to a community's sense of place <input type="checkbox"/></li> </ul>	<b>Guidelines for Exclusion</b> <ul style="list-style-type: none"> <li>is only important to the community for amenity reasons <input checked="" type="checkbox"/></li> <li>is retained only in preference to a proposed alternative <input type="checkbox"/></li> </ul>
<b>E – Research Potential</b> <i>An item has potential to yield information that will contribute to an understanding of the local area's cultural or natural history.</i>	<p>It is anticipated that any machinery associated with the manufacture/assembly of cars in the mid-20th century has been removed in line with the adaption of the factory into a tobacco manufacturing facility and the subsequent occupation by a freight storage company. It is therefore anticipated that there is not likely to be any information comprised within the extant fabric which cannot be gained from other sources.</p> <p>It is beyond the scope of this assessment to assess archaeological potential.</p>
<b>Guidelines for Inclusion</b> <ul style="list-style-type: none"> <li>has the potential to yield new or further substantial scientific and/or archaeological information <input type="checkbox"/></li> <li>is an important benchmark or reference site or type <input type="checkbox"/></li> <li>provides evidence of past human cultures that is unavailable elsewhere <input type="checkbox"/></li> </ul>	<b>Guidelines for Exclusion</b> <ul style="list-style-type: none"> <li>the knowledge gained would be irrelevant to research on science, human history or culture <input type="checkbox"/></li> <li>has little archaeological or research potential <input type="checkbox"/></li> <li>only contains information that is readily available from other resources or archaeological sites <input checked="" type="checkbox"/></li> </ul>
<b>F – Rarity</b> <i>An item possesses uncommon, rare or endangered aspects of the local area's cultural or natural history.</i>	<p>The industrial typology of the site generally is not rare in the context of Sydney generally.</p> <p>There are a number of other examples of former General Motors plants in Australia including one in Marrickville. However, it is appreciated that one of this scale is unique.</p> <p>There a number of better examples of the Inter War Functionalist style in industrial building including the AGM building in Zetland.</p>
<b>Guidelines for Inclusion</b> <ul style="list-style-type: none"> <li>provides evidence of a defunct custom, way of life or process <input type="checkbox"/></li> </ul>	<b>Guidelines for Exclusion</b> <ul style="list-style-type: none"> <li>is not rare <input checked="" type="checkbox"/></li> <li>is numerous but under threat <input type="checkbox"/></li> </ul>

Criteria	Significance Assessment
<ul style="list-style-type: none"> <li>demonstrates a process, custom or other human activity that is in danger of being lost <input type="checkbox"/></li> <li>shows unusually accurate evidence of a significant human activity <input type="checkbox"/></li> <li>is the only example of its type <input type="checkbox"/></li> <li>demonstrates designs or techniques of exceptional interest <input type="checkbox"/></li> <li>shows rare evidence of a significant human activity important to a community <input type="checkbox"/></li> </ul>	
<p><b>G – Representative</b></p> <p><i>An item is important in demonstrating the principal characteristics of a class of NSWs (or the local area's):</i></p> <ul style="list-style-type: none"> <li>cultural or natural places; or</li> <li>cultural or natural environments.</li> </ul>	<p>The subject site is representative of the group of General Motors Holden and the car manufacturing/assembly industry in Australia. The structures on the site are also representative of a group of Inter War Functionalist buildings.</p>
<p><b>Guidelines for Inclusion</b></p> <ul style="list-style-type: none"> <li>is a fine example of its type <input checked="" type="checkbox"/></li> <li>has the principal characteristics of an important class or group of items <input type="checkbox"/></li> <li>has attributes typical of a particular way of life, philosophy, custom, significant process, design, technique or activity <input type="checkbox"/></li> <li>is a significant variation to a class of items <input type="checkbox"/></li> <li>is part of a group which collectively illustrates a representative type <input type="checkbox"/></li> <li>is outstanding because of its setting, condition or size <input type="checkbox"/></li> <li>is outstanding because of its integrity or the esteem in which it is held <input type="checkbox"/></li> </ul>	<p><b>Guidelines for Exclusion</b></p> <ul style="list-style-type: none"> <li>is a poor example of its type <input type="checkbox"/></li> <li>does not include or has lost the range of characteristics of a type <input type="checkbox"/></li> <li>does not represent well the characteristics that make up a significant variation of a type <input type="checkbox"/></li> </ul>

## 4.3. STATEMENT OF SIGNIFICANCE

The subject site is historically associated with General Motors Holden. The plant represents a significant phase of growth in the history of the company and is an exemplar of the growth of the Australian vehicle manufacturing industry generally.



The administration building in the north-east corner of the site is the most intact building (despite changes to internal fitout) and the most prominent from the public domain. The building is also a fine example of the Functionalist style. Characteristics of the style comprised in the building include a predominant horizontality offset by a prominent vertical element (clocktower), pronounced string courses and rhythmic fenestration.

Further, the plant building is indicative of the former function of the site and the remnant Functionalist corner pillars are indicative of the quality associated with the plant.

In accordance with the above assessment it is considered that while the site generally has historic significance, the most significant fabric on the site constitutes the former administration building, and the remnant original and distinctive fabric associated with the plant building.

## 5. IMPACT ASSESSMENT

### 5.1. HERITAGE LISTING

The subject property is not a listed heritage item under the Botany Bay LEP, nor is it located within a heritage conservation area. However, the assessment of heritage significance above has identified that the administration building in the north-east corner of the site has historic, associative and aesthetic significance at a local level. It is also located in the general vicinity of Jellicoe Park (Item 155) and Harris Reserve (Item 66). As such, the below assessment has considered the heritage impact of the Planning Proposal on the identified significance of the site and the proximate heritage items.

Figure 44 – Heritage map indicating the extents of the subject site (blue).



Source – Botany Bay LEP 2013 Heritage Map Sheet 4.

Figure 45 – Heritage map indicating the extents of the subject site (blue).



Source – Botany Bay LEP 2013 Heritage Map Sheet 5.

## 5.2. STATUTORY CONTROLS

### 5.2.1. Local Environmental Plan

The proposed works are addressed in the table below in relation to the relevant clauses in the Botany Bay LEP 2013.

Table 4 – Local Environmental Plan

CLAUSE	DISCUSSION
<p><b>5.10 Heritage conservation</b></p> <p>Note.</p> <p>Heritage items (if any) are listed and described in Schedule 5. Heritage conservation areas (if any) are shown on the Heritage Map as well as being described in Schedule 5.</p> <p>(1) Objectives The objectives of this clause are as follows:</p> <p>(a) to conserve the environmental heritage of Botany Bay,</p> <p>(b) to conserve the heritage significance of heritage items and heritage conservation areas, including associated fabric, settings and views,</p> <p>(c) to conserve archaeological sites,</p>	<p>This report has been prepared in order to consider the Planning Proposal in relationship to the heritage conservation objectives set out in the Botany Bay LEP 2013.</p> <p>In summary, it is considered that the planning proposal allows for the retention of the identified heritage values comprised in the site.</p>

<p>(d) to conserve Aboriginal objects and Aboriginal places of heritage significance.</p>	
<p>(2) Requirement for consent Development consent is required for any of the following:</p> <p>(a) demolishing or moving any of the following or altering the exterior of any of the following (including, in the case of a building, making changes to its detail, fabric, finish or appearance):</p> <p>(i) a heritage item,</p> <p>(ii) an Aboriginal object,</p> <p>(iii) a building, work, relic or tree within a heritage conservation area,</p> <p>(b) altering a heritage item that is a building by making structural changes to its interior or by making changes to anything inside the item that is specified in Schedule 5 in relation to the item,</p> <p>(c) disturbing or excavating an archaeological site while knowing, or having reasonable cause to suspect, that the disturbance or excavation will or is likely to result in a relic being discovered, exposed, moved, damaged or destroyed,</p> <p>(d) disturbing or excavating an Aboriginal place of heritage significance,</p> <p>(e) erecting a building on land:</p> <p>(i) on which a heritage item is located or that is within a heritage conservation area, or</p> <p>(ii) on which an Aboriginal object is located or that is within an Aboriginal place of heritage significance,</p> <p>(f) subdividing land:</p> <p>(i) on which a heritage item is located or that is within a heritage conservation area, or</p> <p>(ii) on which an Aboriginal object is located or that is within an Aboriginal place of heritage significance.</p>	<p>The subject property is not a listed heritage item under the Botany Bay LEP, nor is it located within a heritage conservation area. However, the assessment of heritage significance above has identified that the site generally has some historic significance and that the administration building in the north-east corner of the site and the intact fabric associated with the plant building have historic, associative and aesthetic significance at a local level as the most identifiable and intact components of the site.</p> <p>The site is further located in the vicinity of two heritage items as identified above.</p> <p>As such, this assessment has considered the heritage impact of the planning proposal on the identified significance of the subject site and the proximate heritage items.</p>
<p>(4) Effect of proposed development on heritage significance</p> <p>The consent authority must, before granting consent under this clause in respect of a heritage item or heritage conservation area, consider the</p>	<p>The planning proposal is generally considered to respect the identified heritage significance of the site.</p>



effect of the proposed development on the heritage significance of the item or area concerned. This subclause applies regardless of whether a heritage management document is prepared under subclause (5) or a heritage conservation management plan is submitted under subclause (6).	
<p>(5) Heritage assessment</p> <p>The consent authority may, before granting consent to any development:</p> <p>(a) on land on which a heritage item is located, or</p> <p>(b) on land that is within a heritage conservation area, or</p> <p>(c) on land that is within the vicinity of land referred to in paragraph (a) or (b),</p> <p>require a heritage management document to be prepared that assesses the extent to which the carrying out of the proposed development would affect the heritage significance of the heritage item or heritage conservation area concerned.</p>	This report has been prepared in order to fulfil this condition.

### 5.3. IMPACT ASSESSMENT

The Planning Proposal has been assessed in this section in relationship to the overarching objectives of the Botany Bay LEP 2013.

The planning proposal proposes to change the existing zoning of the site from IN1 General Industrial and R3 Medium Density Residential to R4 High Density Residential. The subject site currently sits in isolation as the only industrial development in a predominantly residential area (despite Eastergardens Shopping Centre to the south). The adjacent site to the immediate south is currently zoned for multi storey residential (R3 - medium density) and is planned for a number of residential flat building. Further, it should also be noted that the subject site is an area strategically accepted for a change in land use. The proposed rezoning is consistent with the strategic vision for Eastgardens as outlined in the Botany Bay Planning Strategy 2031, which envisaged a mixed-use centre in the long term, once BATA had vacated the land.

In accordance with the above, the subject site including the fabric of identified heritage significance will inevitably exist in the context of larger scale development of a different typology than what exists today. This application proposes a residential rezoning which responds to the changing character of the area in terms of scale and typology. The proposed application of residential development across the site will be assessed as part of any future development application for the site's redevelopment.

While the rezoning would facilitate the end of the historic industrial use of the site it should be noted that, the significance of the site is vested in its association with General Motors. The notable historic use of the site therefore specifically constitutes the car manufacturing/assembly industry which moved off the site in the 1980s. The existing generic industrial use i.e. freight storage does not directly contribute to the significance of the site. The requirement to retain its existing industrial use is therefore diminished.

The planning proposal implicitly proposes development in the area currently occupied by buildings associated with the industrial use of the plant including some early fabric. It is understood that the retention of all early elements on site would not provide for its practical redevelopment for residential use. It is considered acceptable that some of this fabric be removed to facilitate the desired use as, despite the

historic associations, much of the early plant fabric was removed under DA2011/272 and the remnant plant building largely comprises standard sawtooth construction with significant areas of contemporary fabric resultant of its conversion into a light industrial facility. It is considered that key identifiers of the site including the administration building could be retained to allow interpretation of its significance, while opportunities for the sympathetic residential redevelopment are explored in other areas. The heritage impact of future residential redevelopment and any removal of buildings would be assessed in detail at the development application stage.

Cognisant of the aesthetic significance and scale of the former administration building to the north-east corner of the subject site, it is proposed to apply a 28m maximum building height along the northern boundary of the site. This includes almost all of the area occupied by the administration building and the area occupied by the distinctive pillars associated with the assembly factory, which are understood to be considered for retention and/or interpretation in the future master plan. This lower scale in the immediate vicinity of the early fabric would serve to minimise visual dominance and to sympathetically moderate the difference in scale between it and the higher development to the south. The zoning across the remainder of the site would allow buildings up to 65m in discreet areas

It is proposed to increase to the maximum floor space ratio from 1:1 to 2.35:1. This would substantially increase the existing density on the site. For the reasons above, it is considered that this may be achievable whilst retaining the intrinsic heritage significance. Support of the proposed maximum floor space ratio is contingent on the appropriate application of bulk across the site i.e. with appropriate setbacks from the heritage fabric. This will be developed and assessed in detail as part of a future development application for the site.

It is understood that the administration building and associated buildings down the eastern boundary of the site are intended to be dedicated to Council for ongoing community/public use. Public access to the site has historically been limited given its industrial use. It is therefore appreciated that transfer of this part of the subject site to public land would allow appreciation of the site's values and interpretation of its former use.

Although no physical works have been proposed under this application, the below constitutes a preliminary assessment of the concept master plan which is submitted as an example of the type of development that could be facilitated by the proposal:

- There is no statutory requirement to retain any of the fabric on the site from a heritage perspective as the site has no statutory heritage listing. However, cognisant of the significance of some identified fabric on the site the concept master plan includes the retention of the administration building and the two northern pillars to the former assembly plant which are the most intact structures on the site;
- The buildings proposed for removal in the concept masterplan constitute those which do not date from the significant original phase of development or those which, while retaining their essential structure, have been highly modified. Specifically, the original warehouse type windows associated with the plant building have been removed and replaced with contemporary fabric which dominate the facades and do not contribute to the presentation of the remnant original fabric. Further, it appears that the characteristic western and southern elevations of the no.2 bond store have been entirely replaced with contemporary fabric of no architectural merit. It is therefore considered that the most intact building is that located in the north-east corner of the site. This is retained in the concept master plan;
- It is understood that there is potential to retain or the remnant pillars associated with the plant building. This will ensure that an understanding of the original scale of the site would be retained;
- It is appreciated that the buildings along the northern boundary of the site are restricted to 8 storeys. This serves to mitigate visual impact on any early fabric retained in that area in terms of scale;
- It is understood that there is an intention to dedicate the land to the north-east of the site to council for community uses. As such, there is an opportunity to enhance public appreciation of the remnant fabric associated with General Motors Holden. It is recommended that a schedule of conservation works be undertaken, particularly for the former administration building, to ensure the integrity of the fabric; and
- There are large public spaces included in the concept master plan. This presents an opportunity to introduce a myriad of interpretation devices which communicate the historic development of the site and General Motors Holden specifically. It is recommended that an Interpretation Strategy be prepared with any future development application for the redevelopment of the site.

## 5.4. HERITAGE OFFICE GUIDELINES

The proposed works are addressed in relation to relevant questions posed in the Heritage Office's 'Statement of Heritage Impact' guidelines

Table 5 – Heritage Office Guidelines

QUESTION	DISCUSSION
<p>The following aspects of the proposal respect or enhance the heritage significance of the item or conservation area for the following reasons:</p>	<p>There are no physical works proposed under this application, as such there would be no physical impact on any early fabric. Any physical impacts proposed as part of a future development application will be assessed in detail in that application.</p> <p>Development along the northern boundary of the site would be required to respond to a lower maximum height than the rest of the site as a result of the planning proposal. This would serve to mitigate visual impacts on any early fabric retained in that area in terms of scale.</p> <p>The subject site is not a listed site. Notwithstanding, it is appreciated that the concept masterplan includes the retention of the most intact and identifiable element on the site, being the administration building and that there is potential to retain or interpret the remnant pillars associated with the plant building. It is recommended that a conservation works schedule is prepared in response to any future DA to ensure the ongoing integrity of the retained fabric.</p>
<p>The following aspects of the proposal could detrimentally impact on heritage significance.</p> <p>The reasons are explained as well as the measures to be taken to minimise impacts:</p>	<p>The rezoning of the site will include the change of use from industrial to residential. While this will end the general historic use it is appreciated that an industrial use in that area is no longer required. Further, it should be noted that the significant historic use is specifically the car manufacturing/assembly industry which has not existed on the site since the 1980s.</p> <p>The Planning Proposal would facilitate development in the area currently occupied by the original buildings associated with General Motors Holden. However, it is understood that the retention of all early elements on site would not provide for the practical redevelopment of the site for residential use. Further, much of the fabric on</p>

QUESTION	DISCUSSION
	site has been highly altered and its original character diminished.
The following sympathetic solutions have been considered and discounted for the following reasons:	N/A
<p><b>Change of use</b></p> <p>Has the advice of a heritage consultant or structural engineer been sought?</p> <p>Has the consultant's advice been implemented? If not, why not?</p> <p>Does the existing use contribute to the significance of the heritage item?</p> <p>Why does the use need to be changed?</p> <p>What changes to the fabric are required as a result of the change of use?</p> <p>What changes to the site are required as a result of the change of use?</p>	<p>The site has had a continuous industrial use since it was developed at the start of the 20<sup>th</sup> century. However, the character of the area generally is changing and the existing industrial use is no longer required in the now suburban area.</p> <p>Further, the historic industrial use which specifically contributed to the significance of the site constitutes the car manufacturing/assembly industry. This use has not existed on the site since the 1980s. The site currently operates as freight storage. Therefore, the requirement to retain the existing use from a heritage perspective is diminished.</p> <p>It is appreciated that it is intended to dedicate the most significant fabric on the site to council. this would ensure that the change of use would not have a physical impact on that fabric. Further, the original use could be more easily communicated through increased public access to the remnant fabric.</p> <p>It is considered that an Interpretation Plan could be developed as part of a future development application which interprets the former industrial use of the site and specifically its connection to the Australian car manufacturing industry.</p>



## 6. CONCLUSION AND RECOMMENDATIONS

The subject site has had a continuous industrial use since it was developed at the start of the 20th century. However, the character of the area generally is changing and the existing industrial use is no longer required in the now suburban area. This application proposes a residential rezoning which responds to the changing character of the area in terms of scale and typology.

Cognisant of the aesthetic significance and scale of the former administration building to the north-east corner of the subject site, it is proposed to apply a 28m maximum building height along the northern boundary of the site. This lower scale in the immediate vicinity of the early fabric would serve to minimise visual dominance and to sympathetically moderate the difference in scale between it and the higher development to the south.

There are no physical works proposed under this application. However, it is appreciated that the concept masterplan includes the retention of the most intact and identifiable element on the site, being the administration building despite their being no statutory obligation to do so in a heritage sense. It is further understood that there is potential to retain or interpret the existing pillars associated with the assembly factory, which would indicate the original scale of the site. It is recommended that a conservation works schedule is prepared in response to any future DA to ensure the ongoing integrity of any retained fabric.

In accordance with the observation set down in this report the Planning Proposal is supported from a heritage perspective.

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*[Note: Some government departments have changed their names over time and the above publications state the name at the time of publication.]*

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